

## CHAPTER 10 REDEVELOPMENT

This chapter provides a redevelopment master and strategic plan for Old Town Flowery Branch. The community visioning process, conducted as part of the Comprehensive Planning process in 2005, resulted in strong support for revitalizing unused or underused blocks in Old Town. This chapter, which was comprehensively rewritten and updated in 2008, is further refined and updated as part of the 2010 update of the Community Agenda.

The 2008 work scope included a revision to the redevelopment boundary in light of updates to the City's historic preservation program. Specifically, it was thought that because preservation and redevelopment activities could clearly come into conflict with one another, the redevelopment boundary should be reassessed in light of any changes proposed to local historic district boundaries.

The two most important blocks of land in need of redevelopment are as follows:

- Block #1. That part of the block (excluding existing buildings fronting on Main Street) bounded by Main Street, Church Street, Chestnut Street and Railroad Avenue; and
- Block #2. That block bounded by Gainesville, Pine, Mitchell and Main Streets.

These two blocks remain the highest priority for redevelopment in the City. Since the 2006 Comprehensive Plan was adopted, the City has completed a major portion of the Main Street Streetscape which includes the entire block between Railroad Avenue and Church Street and approximately one-half block between Church and Mitchell Streets. This changed Main Street, south of Church Street, from two-way travel to one-way travel in a westerly direction (toward Gainesville Street from Railroad Avenue).

One-way travel in the opposite direction will be repeated in the reconstruction of Pine Street between Church Street and Railroad Avenue, which is included in the redevelopment plans for the first redevelopment block (#1 described above).

In addition, the owner of the first redevelopment block (#1 described above) demolished a large part of the building, which had spanned and enclosed Pine Street and extended to Church Street while retaining the intact portion of the storefront facing Railroad Avenue (see photo right).



**Block #1. In 2007, the property owner demolished the dilapidated section (right part of photo) of the building fronting on Railroad Avenue but retained the intact historic building façade (left foreground of photo)**

A major part of the city's initial redevelopment concept for Old Town was the recommendation that the city acquire property and construct a new City Hall and Police Station at the end of Main Street. That recommendation was based first on the need for new police and City hall facilities in Flowery Branch, and second on the opportunity for public investment in civic facilities in Old Town to "anchor" and stimulate redevelopment of the adjacent block (Block #2 described above). Specifically, acquisition of the site north of Gainesville Street at Main Street would also serve a redevelopment function, with the removal of an obsolete structure on that site. As noted in the Community Facilities Element (see Chapter 5), the city purchased two parcels of land in furtherance of its redevelopment strategy and facility plans.

Although Flowery Branch's decision to expand its office space along Main Street has allowed it to defer major decisions about building a new city hall and police headquarters, the purchase of additional land in Old Town now greatly facilitates implementation of the initial plan for a new city hall and police headquarters building to anchor redevelopment in Old Town. Eventually, when new civic buildings are constructed, existing government office space will be moved out of the retail part of Main Street and "free up" space for a more critical mass of retail shops and/or services along Main Street's improved streetscape. The City owns almost the entire block facing the south side of Main Street. This positions the City to become a redevelopment player itself, sometime in the future.

### **TYING PLANNING AND REGULATION TOGETHER**

In Georgia, the Urban Redevelopment Law (O.C.G.A. 36-61) provides that (paraphrased) if the local government adopts a redevelopment plan, it shall in essence be controlling with respect to zoning. Projects consistent with the redevelopment plan are deemed to comply with zoning laws. Specifically, the provision reads as follows:

"Upon the approval of an urban redevelopment plan by a municipality or county, the provisions of the plan with respect to the future use and building requirements applicable to the property covered by the plan shall be controlling with respect thereto." (O.C.G.A. 36-61-7(f)).

That provision of law is not legally applicable here, as this redevelopment element does not constitute an urban redevelopment plan for purposes of complying with the Urban Redevelopment Law (O.C.G.A. 36-61).<sup>1</sup> In principle, it reveals a need to tie any redevelopment plans with the land use, zoning, and development regulations of the City. Similarly, in other states such as California and Oregon, specific plans become binding zoning documents. The consultant and planning staff's familiarity with the concept of a plan binding the zoning process led to the recommendation that the City's CBD zoning district be used as the redevelopment area boundary, and that the CBD zoning district be rewritten to tie into (and make applicable the requirements of) this redevelopment plan element. Hence, the working assumption for the redevelopment area boundary was to make the CBD zoning district and the redevelopment area boundary coterminous (exactly the same).

That working assumption, to make the redevelopment area boundary conform to the CBD zoning district boundary, was formulated when the City received notice from and began working with a private developer with regard to a redevelopment proposal for Block #1 (described

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<sup>1</sup> The issue, of whether the City wanted to prepare and adopt a redevelopment plan that complied with O.C.G.A. 36-61, was discussed between consultant and staff. It was decided that the City did not need to pursue that alternative; Flowery Branch has also availed itself of another redevelopment statute in Georgia, the Redevelopment Powers Law (O.C.G.A. 36-44), thus obviating the need to some extent to follow the older redevelopment statute (O.C.G.A. 36-61).

above), one of the two highest priorities for redevelopment as stated above. The prospects for private redevelopment of a high priority redevelopment block in Old Town presented the opportunity, if not a necessity, for the City to hasten its redevelopment planning effort and also provide the regulatory framework (i.e., the zoning ordinance) that would facilitate the private developer's redevelopment proposal for Block #1. The city completed and adopted revisions to its zoning ordinance in 2008 to facilitate this redevelopment plan.

Certain implications are noted here, with respect to the decision to make the redevelopment area boundary conform to the CBD zoning district boundary adopted on the City's official zoning map. First, any changes to the redevelopment area boundary would be accomplished through changes to the official zoning map (i.e., adding to or taking away from the CBD zoning district). Second, the CBD zoning district was similar to, but did not perfectly match, the preliminary redevelopment area boundaries adopted in the 2006 community agenda document. The CBD zoning district was adopted in a way that also included properties fronting on the west side of Railroad Avenue, south of Old Town proper, all the way to Snelling Avenue. As many of those properties are historically significant residences that have been primarily converted to businesses or offices and most are in good shape structurally, there would not appear to be a need to include them in the redevelopment boundary. On the other hand, including them is not problematic, provided that they do not conflict with preservation objectives and their intended land use and density do not stimulate inappropriate loss of historic resources or intensification of a relatively low-density street (Railroad Avenue).

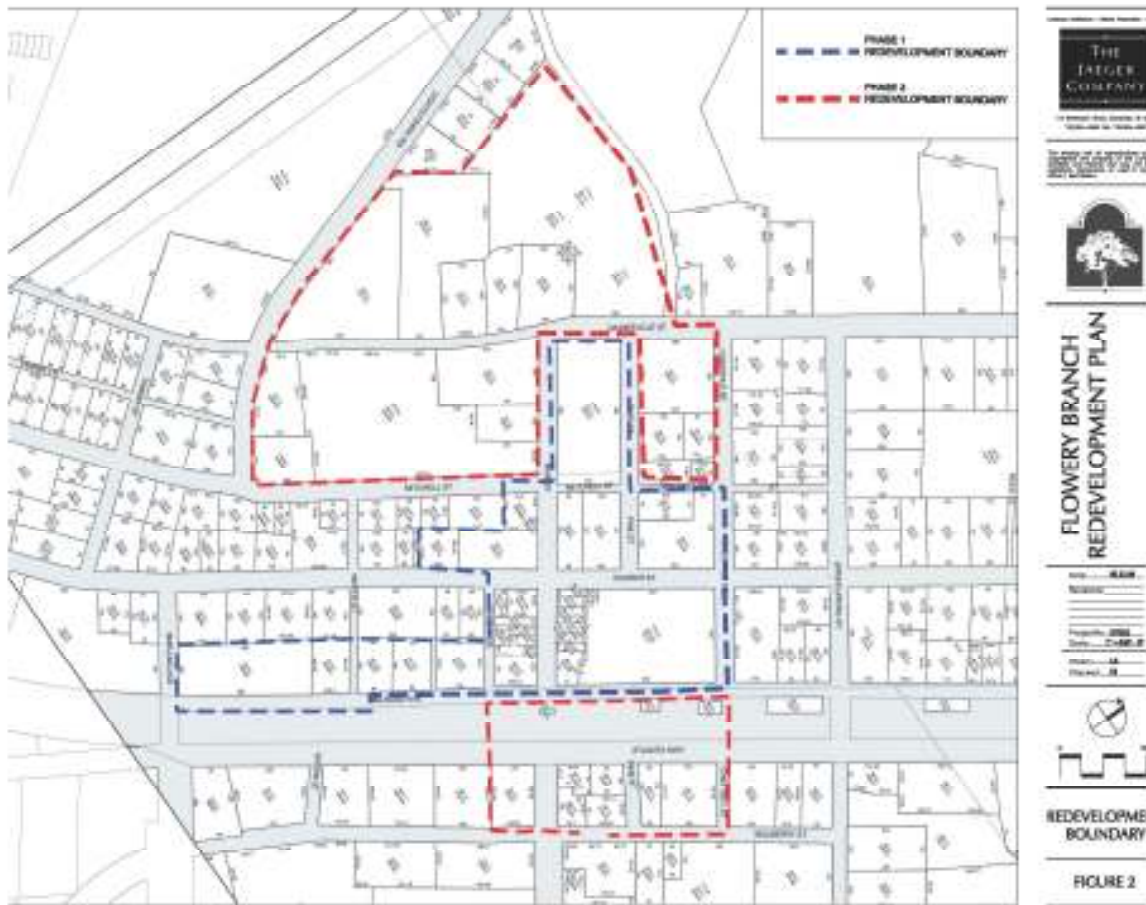
### **PROPERTIES IN THE REDEVELOPMENT AREA BOUNDARY**

The first phase of the redevelopment area boundary is the same as that in the CBD, Central Business Zoning District (See Figure).

During the process of reviewing this redevelopment element in 2008, it was determined that additional properties outside the Phase 1 redevelopment area boundary were possible. It was decided that, rather than exclude the properties determined previously as appropriate for redevelopment because they were not zoned CBD, that a "phase 2" redevelopment area boundary would be added to the map.

Phase 2 includes properties west of Mitchell Street south of Main Street to Lights Ferry Road, properties west of Gainesville Street, and properties east of Phase 1 which front on Atlanta Highway (SR 13), directly across the railroad from the phase 1 redevelopment area boundary. In addition, some of the properties west of Mitchell Street south of Main Street to Lights Ferry Road were added, given consideration of the proposed extension of Lights Ferry Road to Snelling Avenue as called for in the Downtown Transportation Study. Planners thought that that road improvement will open up additional access to Old Town, and that these parcels represent good potential for redevelopment opportunities in the longer term.

Properties in the Phase 2 boundary are not intended to be redeveloped in the short term, and they are not zoned CBD at the time this comprehensive plan amendment is adopted. It is intended that Phase 1 redevelopment occur prior to Phase 2 redevelopment. However, at any time appropriate, a property owner in the Phase 2 redevelopment area boundary can propose a redevelopment project and apply for CBD zoning. The future development (character area) map and future land use plan map have been revised to reflect phase 2 properties as eventually appropriate for inclusion within the Old Town character area and the central business district, respectively, on those two maps.



### **FUTURE LAND USE AND DENSITIES IN OLD TOWN**

This Community Agenda provides a statement of vision and some development parameters for Old Town (See Chapter 9, “Old Town” and also “Mixed Use, Downtown”). In particular, the description of the “Old Town” character area indicates recommended intensities for development. Similarly, the Comprehensive Plan’s description of the “Mixed Use, downtown” character area also establishes recommendations for development intensities.

### **ZONING OF OLD TOWN**

Phase 1 of the redevelopment area boundary is zoned CBD, Central Business District, according to the adopted Official Zoning Map of Flowery Branch. As with any other zoning district, the CBD regulations establish use limitations and also “dimensional” requirements that regulate height, intensity, and building placement. The height limitation established in the CBD zoning district and applicable to Old Town was a maximum of three stories, and an overall height limit of 45 feet was established. That height limit in the CBD zoning district was amended concurrent with a comprehensive plan amendment in 2008 to enable taller buildings on certain core blocks as contemplated in this redevelopment element. Also, floor-area ratios were adjusted upward in this plan amendment and in the CBD zoning district regulations to

accommodate more intensive development for priority redevelopment blocks and properties along Main Street between Railroad Avenue and Church Streets.

There is no maximum lot coverage established in the CBD zoning district – buildings can under the zoning regulations cover the entire property with building footprint.

### **APPROVED REDEVELOPMENT PROJECT FOR BLOCK #1**

As alluded to above, the City in 2007 received a preliminary development proposal by private developers to redevelop Block #1 described above, one of two high priority redevelopment blocks identified in the redevelopment element adopted in 2006 and reiterated here in this amended redevelopment element. Following the City's historic district regulations, the developers sought and received approval from the Flowers Branch Historic Preservation Commission in 2007 to demolish the obsolete structure on Block #1. The developer then presented conceptual plans for a redevelopment project which were largely in keeping with the recommended land uses shown on the preliminary development concept (see figure 1), but at a higher intensity (a FAR of 1.81 excluding the parking structure) than that contemplated in the Old Town character area as described in this Community Agenda.

Over a series of months in 2007 and 2008, with meetings and significant back and forth communication, the developer and city staff arrived at what was thought to be an acceptable redevelopment project from the overall standpoint of architecture, preservation, and land use compatibility. The redevelopment project gained the approval through a Certificate of Appropriateness issued by the Flowers Branch Historic Preservation Commission.

### **USES IN THE REDEVELOPMENT AREA**

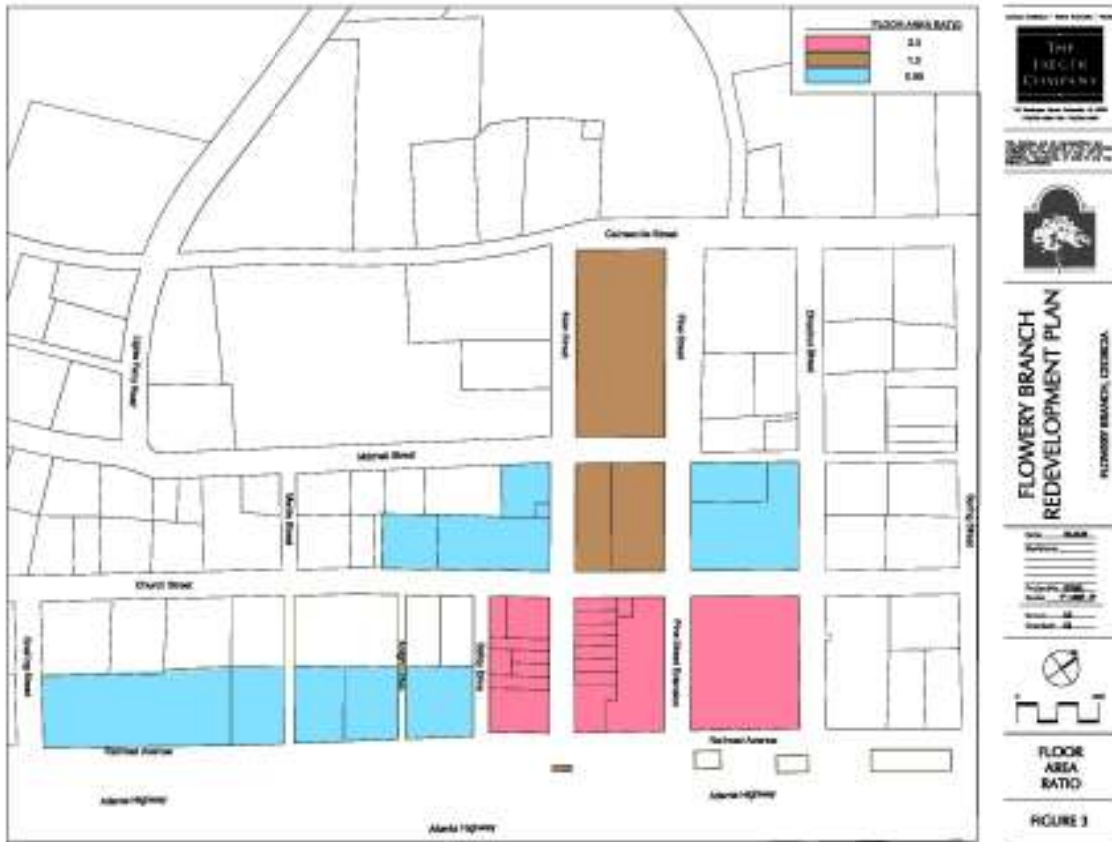
Uses permitted in the CBD, Central Business District zoning district, are specified in Flowers Branch's zoning ordinance. The preliminary redevelopment proposal (initially adopted in 2006 but excluded here) assigned recommended land uses to properties within the preliminary boundary. Consultants contemplated whether the revised redevelopment element should propose uses for each property and show desirable building footprints. It was decided during the process that such specificity might constrain the ideas of property owners and future developers who might view the recommendations as binding, or that might be unattainable if market conditions differed at the time of redevelopment. Therefore, this revised redevelopment element does not assign desirable or recommended land uses; instead, use decisions will be guided by the permitted uses section of the CBD zoning district. However, policies for redevelopment and some illustrative guidance are provided in this chapter.

### **BUILDING INTENSITY AND HEIGHT**

All of the properties in Phase 1 of the redevelopment area boundary are governed by the use regulations of the CBD Zoning District. The CBD zoning district's dimensional requirements were amended in 2008, concurrent with adoption of this revised redevelopment element in 2008, to provide for greater intensities on blocks fronting along Main Street and Pine Street Extension between Railroad Avenue and Church Street. The plan calls for development intensities to be reduced as one moves away from these core blocks of the redevelopment area.

Floor Area Ratios are shown graphically on a figure for properties within the Phase 1 redevelopment area boundary, which was also made a part of the CBD zoning district via

zoning ordinance amendment in 2008. The map shows different colors to illustrate the different FARs.



## **CHARACTER AND DESIGN**

### **Streetscape Improvements**

Streetscape improvements along Main Street between Railroad Avenue and Church Street have set a positive framework for future streetscape extensions in accordance with the designs set in that project. The design of Main Street has already influenced the design of Pine Street Extension, has been integrated into redevelopment plans for Redevelopment Priority Block #1.

Decorative pedestrian-orientated site elements and streetscape elements should be used to provide visual continuity with the existing Main Street streetscape. The standards established in the recent Main Street project should be used or, if not used, pedestrian amenities that are complementary to existing amenities should be used. Such streetscape elements include benches, trash receptacles, pedestrian light fixtures, bike racks, street trees, and decorative crosswalks to insure pedestrian safety at roadway crossings. Also pedestrian and bikeway recommendations called for in the Metropolitan Planning Organization's bicycle and pedestrian plan should be consulted for their inclusion in future citywide pedestrian improvement projects as well as within private developments.

### **Historic Resources**

Simultaneous with the update of this redevelopment element, the City of Flowery Branch prepared a revised historic resources inventory and reevaluated and reestablished its historic district (see Chapter 8 of this Community Agenda). Within that part of the redevelopment area boundary covered by a locally established historic district, the Flowery Branch Historic Preservation Commission has jurisdiction to review all material changes in appearance to buildings, structures and grounds. It does so through the review and issuance of Certificates of Appropriateness. There are therefore some safeguards that redevelopment within the local historic district will preserve historic resources and that new development will be sensitive to (and compatible with) the existing historic character of Old Town.

### **Update of Central Business District Zoning**

Also as a part of the update to this redevelopment element in 2008, the City prepared amendments to its Zoning Ordinance to facilitate the redevelopment of Redevelopment Priority Block #1 and other properties within the redevelopment area boundary. The CBD zoning district implements a discretionary review process, called Redevelopment Plan Approval, which offers the opportunity for the City to review the site plan and architectural details of future redevelopment projects. During that process, redevelopment projects will be scrutinized for details regarding the layout of the redevelopment blocks, the uses and buildings proposed, and the architecture of buildings.

### **TRANSPORTATION IMPROVEMENTS**

Old Town Flowery Branch contains narrow streets for the most part which will not support extensive intensification of land uses through redevelopment. As a part of this redevelopment planning effort, the existing road rights of ways have been analyzed and some recommendations are made here with regard to additional right of way acquisition (preferably, donation at the time of redevelopment) that will be needed to ensure road improvements that match the circulation pattern required for redevelopment of Old Town.

One of the most important road improvements in Old Town is the extension of Pine Street in a southerly direction between Railroad Avenue and Church Street. This will be a one-way street in a southbound direction. This road improvement project is important as Main Street is one-way northbound, and with the proposal for Pine Street Extension to be one-way in the opposite direction, upon completion this will provide for pedestrian and vehicular circulation in the downtown core. Within block #1 of the redevelopment area boundary, the proposed streetscape for Pine Street Extension is to replicate the design used for Main Street. This is important as the Main Street segment between Church Street and Railroad Avenue was designed with a great deal of flexibility. In the future, if less on-street parking is desired, the existing angled spaces can be replaced with parallel parking and thus space is available to add a median or return two-way travel lanes. Pine Street Extension should offer that same flexibility.

Each property redeveloped in the redevelopment area of Old Town is expected to install sidewalks and streetscape features as illustrated on the Redevelopment Master Plan, at the time of redevelopment. The two types of streetscape sections shown in the redevelopment plan are best illustrated in the recent streetscape improvements on Main Street. The first section between Church Street and Railroad Avenue includes a continuous sidewalk extending from the curb to the front facades of the storefronts, Vegetation is provided by tree wells set into the sidewalks. In contrast the second type is the recently completed section between Church and Mitchell Streets which includes a curb, a green buffer with grass and trees ranging from 8-9 feet

wide, and then the sidewalk. Recognizing the limited public right-of-way within the City's narrow streets, the green buffer can range in width with 4-5' recommended as a minimum.

### **Railroad Right-of-Way**

The railroad right-of-way between Atlanta Highway and Railroad Avenue is owned by the railroad except for a few small tracts. Flowery Branch is limited in terms of what it can do within the private right-of-way of the railroad. Presently, Flowery Branch has permission to use part of the railroad right-of-way on the west side (abutting Railroad Avenue) for off-street parking. The City should maintain existing arrangements and expand them where possible to facilitate the expansion of off-street parking opportunities as redevelopment occurs. The City should also explore other options for beautification of this open space, such as wildflower plantings that would require limited maintenance and add seasonal color to the downtown district.

### **Additional Infrastructure Support for Redevelopment**

One of the most widely accepted tools used to promote redevelopment is the provision of infrastructure (i.e., streetscape improvements, road improvements, off-site and on-street parking and storm water management facilities, etc.). Flowery Branch has already helped to facilitate redevelopment with implementation of the Main Street streetscape project, and a second phase streetscape project promises to add further to the infrastructure support for the redevelopment of Old Town. Additional public investment in infrastructure is desirable in order to help facilitate redevelopment. Such investment signals interested developers that the local government is committed to the revitalization of the area.

Since the City has done a good share of streetscape improvement by installing new streetscapes along Main Street between Railroad Avenue and Church Street, and since this plan calls for developers to install road improvements along existing street frontages at the time of redevelopment, where called for in this plan, the City of Flowery Branch should focus any future resources (beyond the second phase of streetscape improvements) to ensure adequate water and sewer lines (and treatment capacities), install drainage improvements, provide public parking facilities in conjunction with private parking on redeveloped properties, and partnering to install Old Town gateway features.

### **REDEVELOPMENT OF CITY-OWNED PROPERTIES**

Because the City owns significant properties in the downtown area, it may become a major player itself in future redevelopment activities. In particular, the properties along the south side of Main Street might be redeveloped in the future at a higher intensity and scale that matches existing development on the north side of Main Street. The City's involvement in redevelopment of the south side of Main Street is considered long-range.



## **GUIDELINES FOR REDEVELOPMENT**

Following are guidelines to consider in proposing redevelopment projects. These guidelines will be considered during the process of redevelopment project approval. Inconsistency with them may be cause for disapproval of a redevelopment project.

### **Protect Historic Character**

The first consideration in redevelopment is to assess the potential historic status of all the buildings on the site and the current condition with the intent to initially consider reuse options for the existing buildings. The Historic Resources Inventory (dated March 2008) may reveal opportunities for the preservation of historic buildings. There might be the potential to utilize Historic Preservation Tax Credits in a future project if the office building and possibly others were deemed eligible for the National Register of Historic Places. Utilization of historic tax credits would require the developer to do a "Certified Tax Project" following the Secretary of Interior Standards for Rehabilitation Projects.

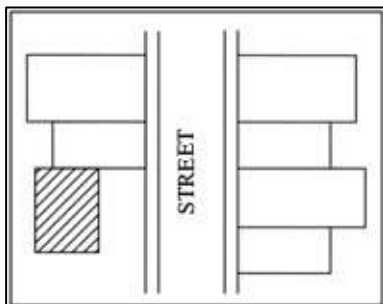
### **Block Size and Width/Length**

Some of the blocks in the redevelopment area boundary are short and narrow, with some block widths less than 200 feet and others averaging approximately 300 feet. As redevelopment of blocks within Phase 1 or Phase 2 of the redevelopment area boundary occurs, the development should take place on subdivided blocks of land that are no more than 300 feet in block width, unless it can be shown that a larger block configuration is needed to facilitate the redevelopment proposal. To the extent possible, the pattern of small city blocks should be retained as redevelopment occurs.

### **Building Frontage**

New building construction must front on an existing street. The main entrance of new construction cannot be located on a side elevation or rear elevation; it must face Main Street or Pine Street.

### **Building Elevations**



In this sketch, the proposed buildings do not maintain the established setback of the historic building (hatched). It would be inappropriate for any redevelopment project to not maintain established setbacks in Redevelopment Blocks following the prevailing standards of adjacent blocks.

The elevations of buildings should be enlivened by architectural features so that a blank elevation (defined as horizontal or vertical distance on an elevation exceeding 15 feet without an architectural feature) is not presented to the residences and passersby along streets. Architectural details that do not belong to the period or style of the district are not appropriate for

detailing on new construction. Architectural details should be determined from the project's area of influence, as well as those found within the district, and must promote architectural interest. These features must be balanced and not create a cluttered appearance. Exterior materials for one to two story building construction may be of wood siding or masonry, or a combination of the two. Exterior materials for infill construction in excess of two stories shall be of masonry construction.

### **Building Setbacks**

Building setbacks for redevelopment should maintain the existing setback established by existing historic buildings. For instance, in Priority Redevelopment Block #2, construction should front on Main Street, and maintain the setback of the existing office building (historic structure) unless this portion of the property is demolished. All redevelopment should follow the prevailing setbacks of the adjacent blocks.

### **Sidewalks**

Sidewalks need to be provided along all streets. Sidewalks should also be provided within the site and along primary building frontages connecting open spaces, buildings and parking lots with the existing sidewalk network found in Flowery Branch and to the buildings.

### **Parking Lots**

Parking lots should be divided into smaller parking areas and "softened" by planted tree islands or other landscape treatments. A rule of thumb is as follows: trees planted within and on the periphery of parking lots must be of sufficient canopy to provide shade to pedestrians and vehicles at maturity.

### **On-street Parking**

On-street parking should be provided where permissible by zoning and other regulations.

### **Off-street Parking**

Off-street parking should not front on the right-of-way, but rather, it should be placed to the rear (preferred) or side of buildings. Placing parking lots on the interior of this block is encouraged.

### **Commercial Uses in Residential Structures Along Railroad Avenue**

Within the CBD zoning district and the redevelopment area boundary are one-story residential historic structures that have been converted to businesses and offices. It is critical, in maintaining existing character, to retain these historic residential structures. Furthermore, it is desirable that the existing historic structures along Railroad Avenue be retained, and that expansions are made through reuse or redevelopment that are historically compatible with the existing residential dwellings. Because the development pattern of these residential historic structures is different from that contemplated in Redevelopment Priority Blocks #1 and #2, it calls for some different standards to guide their redevelopment. Recommendations to consider in the future redevelopment of the historic structures on Railroad Avenue are provided below; they begin first with guidelines for the preservation of existing structures, followed by guidelines for new or infill construction:

- Rehabilitated residential buildings that are converted to a commercial use should follow the residential design guidelines and practices established by the Historic Preservation Commission. Follow the U.S. Department of the Interior's Standards for Rehabilitation Projects.
- Proposed new uses for residential buildings should be compatible with the historic property so that minimal changes are necessary. When adaptive reuse is complete, the property must still be recognizable as a residential property.



The structure in this photograph has been converted to a restaurant. The façade of the structure has been preserved, allowing its original form to be clearly recognizable.

- Arrangement and symmetry of the front facade should be preserved during any adaptive use project.
- Historic porches located on the side, or rear may be enclosed when establishing a new use. Historic porches located on a primary elevation should not be enclosed. The enclosure of the porch must be carefully designed in a manner that preserves the historic character and form of the original building. This can include using large sheets of glass and recessing the enclosure wall behind existing scrollwork, posts, and balustrades.
- Additional entrances, if required for a new use, should be constructed in a manner that preserves the historic character of the building, such as limiting such alteration to non-character-defining elevations.
- Additional windows on rear or other-non character-defining elevations, if required by the new use, should be compatible with the overall design of the building.
- Dropped ceilings, if required on the interior by a new use, should not block the full-height view of a window opening from the exterior.
- Additional stories, when required for a new use, should be designed to be set back from the wall plane and should be as inconspicuous as possible when viewed from the street.
- Historic additions and alterations that have acquired significance in their own right should be preserved when feasible.



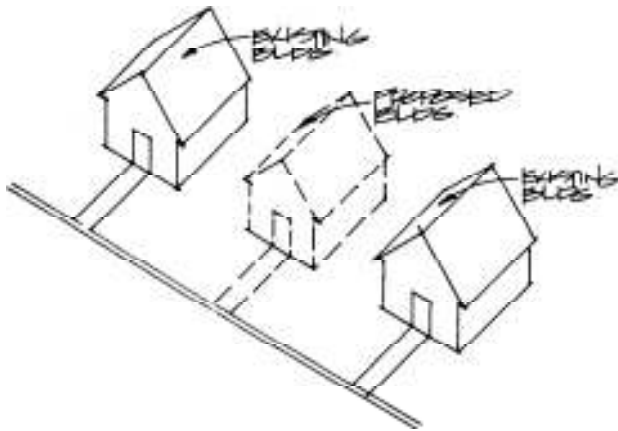
The historic clipped-roof bungalow (far left) has been engulfed by a right side addition that inappropriately extends beyond the original front façade; this addition is out of scale with the original building.

- Additions should be designed to have the least effect possible on historic materials or character-defining elements of the historic building or landscape.
- Additions should be placed on the rear or on an inconspicuous side of the historic building, and they must be compatible with the original building materials, relationships of solids to voids, and color. The size and scale of the addition should be limited in relationship to the historic building.
- Additions to the side of a historic building should not be flush with the front facade. Appropriately-designed side additions to historic buildings will be stepped back from the front facade, as far back as possible.



The proposed addition to this building is placed on the rear elevation, is clearly differentiated from the original structure, and is appropriately scaled.

- Design of a new addition must be clearly differentiated so that the addition is not mistaken for part of the original building and also does not damage the historic fabric of the existing building. Additions that could be easily removed, at a later date, are encouraged by the construction of breezeways, open or enclosed.
- Historic landscape features of the property should be preserved and maintained despite a change in use of the property, whenever feasible.
- Setback for all construction in the redevelopment block should maintain the existing and prevailing setback established by the historic resources fronting on Railroad Avenue.



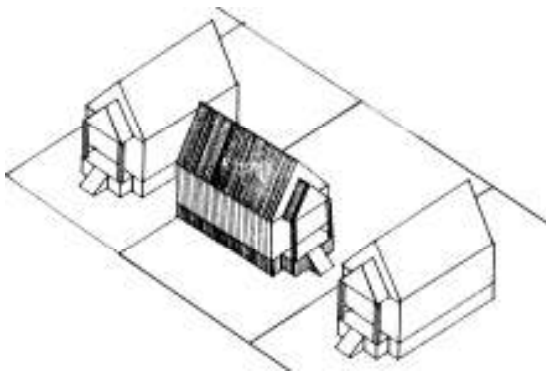
As seen in this illustration, the proposed building (middle) follows the prevailing setback established by adjacent structures

- New construction should follow the established patterns of existing buildings, including roof pitch, fenestration, materials, scale, and texture.



The house on the left (top and bottom) is existing. The fenestration on the proposed house on the top right does not follow the pattern established by the existing house. The proposed house on the bottom right does follow the established fenestration pattern.

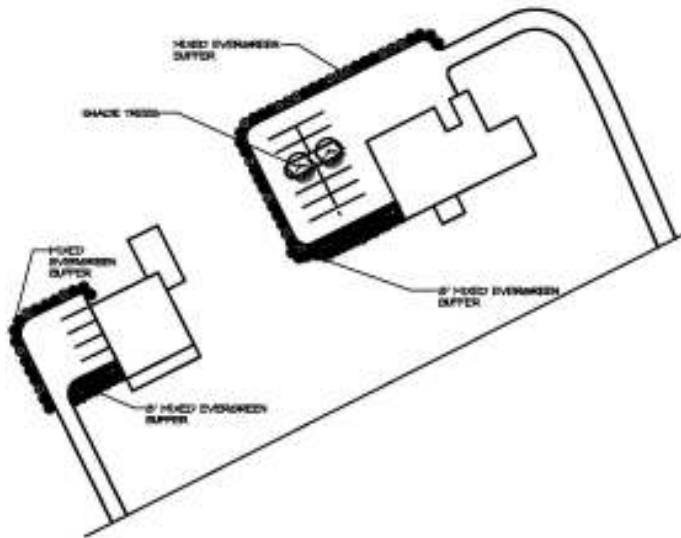
- New construction should follow the prevailing orientation of existing buildings on the adjacent block(s).



The orientation of the proposed building (middle) does not follow the orientation established by the flanking structures.

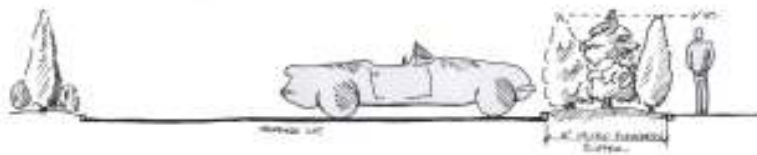
- Sidewalks need to be provided along Railroad Avenue and within parking lots for infill construction that connect with the existing sidewalk network found in Flowery Branch and to the buildings.

- Parking lots should not be placed within the prevailing setback of the existing structures on Railroad Avenue.



The parking lots in this illustration are appropriately located to preserve the prevailing setbacks of the structures.

- Off-street parking should be sited to the rear (preferred) or side of the infill construction. Such parking should be buffered from the public view from Railroad Avenue by landscaping, landscaping berms, planting strips, or low walls or fences of a historically appropriate design.



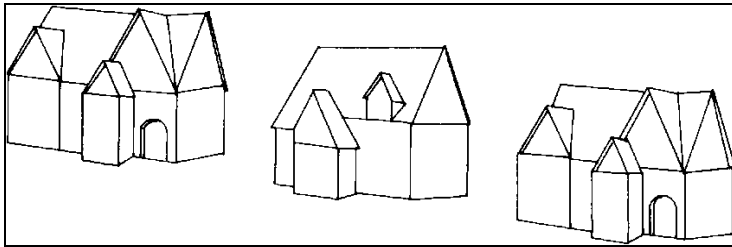
This section view illustrates appropriate methods of buffering a sidewalk, from Railroad Avenue, including a planting strip and a landscaping berm.



Landscaping, as seen in this photograph, defines the parking lot edge and provides added visual definition between the pedestrian and vehicular realms.

- Architectural details should be obtained from the project's immediate area, as well as those found within the district, to promote architectural interest on new buildings. These features must be balanced and not create a cluttered appearance and should be residential in style.

- Exterior materials for infill construction within this redevelopment block should be of wood to complement the existing historic buildings.
- Massing for infill construction should be residential in scale and should draw from the historic housing stock.



The infill construction (middle) appropriately draws from the residential character of the adjacent historic residences.